



County of Fairfax, Virginia

**2011 Planning
Commission**

December 12, 2011

Peter F. Murphy, Jr.
Chairman
Springfield District

Hillary Zahm, AICP
Senior Manager, Development
Macerich

Walter L. Alcorn
Vice Chairman
At-Large

1961 Chain Bridge Road; #105
McLean, VA 22102

Suzanne F. Harsel
Secretary
Braddock District

**RE: Architectural Review of Parking Garage F-1
and Phase I, Tysons Corner Center, Macerich,
per proffers executed with RZ/FDP 2004-PR-044**

Frank de la Fe
Parliamentarian
Hunter Mill District

Dear Ms. Zahm:

This will serve as your record of the Planning Commission's action on the architectural review of parking garage F-1 and Phase I of Tysons Corner Center, as per RZ/FDP 2004-PR-044, Providence District.

Jay Donahue
Dranesville District

On Thursday, December 8, 2011, the Planning Commission voted unanimously (Commissioners Hall and Harsel absent from the meeting) to support the architectural materials submitted by the applicant, dated November 8, 2011, for the Macerich parking garage F-1 façade, the Phase I office building, and the landscaping along Route 123, as meeting partial commitments in Proffers 17, 18 and 20 executed with the approval of RZ/FDP 2004-PR-044.

Earl L. Flanagan
Mount Vernon District

Janet R. Hall
Mason District

James R. Hart
At-Large

Attached for your information is a copy of the verbatim excerpt from the Planning Commission's action on this application. Should you need any additional information on this action, please do not hesitate to contact me at 324-2865.

John L. Litzenberger
Sully District

Kenneth A. Lawrence
Providence District

Sincerely,

James T. Migliaccio
Lee District

Timothy J. Sargeant
At-Large

Barbara J. Lipka
Executive Director

Barbara J. Lipka
Executive Director

Attachment (a/s)

cc: Linda Smyth, Supervisor, Providence District
Ken Lawrence, Commissioner, Providence District
Peter Braham, Zoning Evaluation Division, DPZ
December 8, 2011 Date File
O-8c File

Dawn M. Ashbacher
Assistant Director

Kara A. DeArrastia
Clerk to the Commission

 To request special accommodations, call the Planning Commission office at 703-324-2865, TTY 703-324-7951. Please allow seven working days to make the appropriate arrangements.



Planning Commission Meeting
December 8, 2011
Verbatim Excerpt

ARCHITECTURAL REVIEW OF PORTIONS OF PHASE 1 OF TYSONS CORNER CENTER
(MACERICH)

During Commission Matters

Chairman Murphy: Mr. Lawrence, do you want to continue?

Commissioner Lawrence: Thank you, Mr. Chairman. Is there any other matter?

Chairman Murphy: No.

Commissioner Lawrence: Okay. I have an administrative item to do. I think everybody got –

Chairman Murphy: That one.

Commissioner Lawrence: – a copy of this package from Macerich. And I've asked Commissioners whether they had any questions. We have with us tonight the architects, you know, who did this work and they're open to answer questions real time. Has anyone any questions on this material?

Chairman Murphy: Are there any question on the architectural plan? Apparently not.

Commissioner Lawrence: Mr. Migliaccio, did you get your question attended to earlier?

Commissioner Migliaccio: I did. They were very –

Commissioner Lawrence: Do you think that might be of general interest?

Commissioner Migliaccio: Sure, I can bring it up now too.

Commissioner Lawrence: Thank you. Please do.

Commissioner Migliaccio: I had just one general question regarding your green screen. Can you just explain how this -- how it functions on this site and how big the pieces are and how it will work?

Jared Krieger, Designer and Project Architect, Gensler: Sure, my name is Jared Krieger. I'm the – one of the designers and project architects and I work with Gensler, who's the architect on the project. Thank you for the opportunity to let me answer some of your questions.

Chairman Murphy: Thank you.

Architectural Review of Portions Of Phase I of Tysons Corner Center (Macerich)

Mr. Krieger: The green screen is a product that we're specifying. It's specifically called green screen. It's a three-dimensional space frame and it gives opportunity for vines to grow in multiple different patterns and it comes in modular sizes ranging from five feet to ten-foot lengths, but if you notice in the design package – the design that we have specifically gone for is for larger pieces. We didn't want these to look like little backyard trellises in smaller pieces. So the intent is in the long term to have the growth cover the larger mass to make larger, more bold moves and make it more consistent.

Commissioner Migliaccio: And what is this hiding? What is behind it?

Mr. Krieger: Behind it is a parking structure.

Commissioner Migliaccio: Okay. Thank you.

Mr. Krieger: Thank you.

Commissioner Lawrence: Mr. Chairman, I too have one question for the architects. We have here a high-rise office building – the Phase I office building. Could you speak to the point of making high-rise buildings bird-friendly in whatever terms you can with this design?

Mr. Krieger: Thank you, that's a good question. Early in the design process we had set as a team with both the client and the design team our sustainability goals and that was one of the concerns that has come up. We've worked to great lengths to balance the strict energy requirements of both the Building Code and LEED and our sustainability goals and when it comes to bird-friendly buildings, a big part of that is the glass type. And I think we all know that glass has a little bit of reflectivity no matter what and it just inherently – birds see glass and they see that as a continuation of their environment. And we've worked with Viracon, who's one of the leading world's leading glass manufacturers, and they've done some pretty extensive studies with the American Bird Conservancy – I'm sorry, Conservatory. And the thing that we find that has the most successful – makes the buildings most successfully bird-friendly – is creating a visual noise on the façade. And we've gone through great lengths on the design team to balance the reflectivity and the visual noise. Such things as frit and through their studies they've specifically found that when you have a horizontal frit on the façade in the glass, that's the most – one of the most effective deterrents to birds. And we have frit on some of our glasses. We've done vertical blades on some of the skins to add just more of a texture to the skin so that it's not just a larger sheet of glass. Sun shades, we have on one of our larger facades, adding more texture and visual noise. We've made some larger mass moves and all of the base building will be installed with blinds and all these – with roller shades. They're called "mecho shades." And all of these – all of these moves create more visual noise and that's been one of the biggest deterrents at making the building bird-friendly. And finally we've – we've just want to touch a little bit on building lighting because that has a little bit of an impact on birds and all of our building lighting – we're shielding from illuminating the night sky. And that's a specific goal of ours from the beginning was to make sure that we're not illuminating the sky because there are some species of birds that use stars for navigation and when you start to illuminate atmosphere around that it deters the

Architectural Review of Portions Of Phase 1 of Tysons Corner Center (Macerich)

birds and it just creates an unhealthy environment for them. So we've – we've used all those specific sustainability goals to address a bird-friendly building.

Commissioner Lawrence: Thank you very much. There are three items where – Commissioner Flanagan?

Commissioner Flanagan: Yes, I had a question for the speaker as well.

Commissioner Lawrence: Please.

Commissioner Flanagan: On page 14 of your presentation, you show a green roof. But there's very little in the text that explains what's occurring in that space and there seems to be about – more paved surface up on the roof than there is green roof.

Mr. Krieger: Yes.

Commissioner Flanagan: I would like to know, you know, why you arrived at the square footage that you are going to have up there and what the – the landscaping plan shows all the plants at the ground level, but there's nothing about what's producing the green up on the green roof.

Mr. Krieger: Sure, that's a good – fair question. On the green roof we've specifically targeted the southern exposure. We've done a lot of green roofs in the area and find that we get the most success in the southern exposure when they have direct sunlight. So that specifically addresses why we put mechanical equipment on the north side of the penthouse and kept the green roof on the south side of the penthouse. Some of the areas are required around the green roof just to get to façade access for maintenance and window washing and cleaning, and we're specifying a product that comes in a roll of sheets. And it's just – it's a Sedum product. And it just has a mixture of wild flowers and seeds and it serves as a – it helps us retain water in these specific areas with these more wild flowers. And we've had a lot of success with that in the past as opposed to trying to specifically landscape just more specific things like shrubs and specific flowers and plants.

Commissioner Flanagan: So there's one little wash room up there with a – it looks like one wash – one water closet –

Mr. Krieger: Yes.

Commissioner Flanagan: – and one lavatory. I take it that you're not planning to have the roof deck used for employee recreation of any sort? Or the green space?

Mr. Krieger: The green space itself will not be occupiable. That would be – that would be a safety concern so we're keeping people to the open roof terrace area, which keeps the occupant count a little bit lower. And specifically within the Building Code we use a lot of the fixture

Architectural Review of Portions Of Phase 1 of Tysons Corner Center (Macerich)

counts on the floors below to account – accommodate some of the occupants that are on the upper level.

Commissioner Flanagan: Sure. One of our hopes, of course, in Tysons which is short of recreational space is that there would be using some of the roofs – maybe at the lower buildings might be more appropriate than a sky scraper, but certainly – it would be difficult to get a ball retrieved from going over the parapet.

Commissioner Alcorn: Mr. Chairman?

Mr. Krieger: And we certainly have a beautiful view up there.

Chairman Murphy: Mr. Alcorn.

Commissioner Alcorn: Yes, actually –

Hillary Zahm, AICP, Senior Manager, Macerich: Do you mind if I mention one thing, there's one last –

Commissioner Alcorn: Please, and then I'm going to ask you a question.

Ms. Zahm: Oh, okay. Oh, oh. Hillary Zahm with Macerich. On Sheet 18, you'll see a great deal of landscaping shown. I just want to mention that while some of that landscaping isn't grade, a significant portion of it is actually on our parking podium. So there actually are – there's more than one green roof here. And this is probably more likely where you'd throw a ball and hopefully you wouldn't throw it out onto Route 123, but that gives you a lot more open space – more usable open space for the public.

Commissioner Flanagan: Excellent.

Commissioner Alcorn: Mr. Chairman?

Chairman Murphy: Mr. Alcorn.

Commissioner Alcorn: Ms. Zahm, when are you going to start building?

Mr. Krieger: That's a good question.

Ms. Zahm: We're hoping to get underway sometime next year.

Commissioner Alcorn: Okay.

Ms. Zahm: Do you have any tenants that are looking for space in Tysons Corner, Commissioner Alcorn?

Architectural Review of Portions Of Phase I of Tysons Corner Center (Macerich)

Commissioner Alcorn: No, unfortunately not, but –

Ms. Zahm: No, we really are hoping to get started pretty soon. We're in the middle of site plan and that's why we're here before you today. We want to keep going forward.

Commissioner Alcorn: Great. Thank you.

Ms. Zahm: Thank you.

Chairman Murphy: Mr. Lawrence.

Commissioner Lawrence: Thank you, Mr. Chairman. It's good that we have this last visual up because there are three items we're looking at. One of them is the F1 garage façade. That was what was behind the green wall that Commissioner Migliaccio asked about. The other is the office building and that was my question about being – it being bird-friendly. And the third is the landscaping along 123, which you see in plain view here. There are also some perspectives of it inside. I'm sure everybody saw that when they looked through it. I didn't notice anybody with any questions about it. We do have the landscape architect here if anybody has any questions.

Commissioner Alcorn: Mr. Chairman?

Commissioner Lawrence: Hearing none?

Commissioner Alcorn: Mr. Chairman?

Commissioner Lawrence: Mr. Alcorn.

Chairman Murphy: Mr. Alcorn, sorry.

Commissioner Alcorn: Yes, just very quickly if that graphic could be put back up? This helps illustrate, I think, something not for this site, but on other sites where – particularly the building on the right that has a straight frontage – the road is not straight. The road is nice and curved. That makes it kind of difficult. You know, I think that probably presents you guys with – well, I'll ask you. Does that present some challenges in creating urban architecture with a nice curved road frontage like that?

Mr. Krieger: Does the curve create a challenge?

Commissioner Alcorn: Yes.

Mr. Krieger: I think it does. You look around – especially in Fairfax, curves are done a lot and some more successful than others – and we specifically went through the effort to just sort of flatten the building and angle it away from the street, opening up that public space a little bit

Architectural Review of Portions Of Phase 1 of Tysons Corner Center (Macerich)

more. And if you look at the elevation, we've indented some of the pedestrian zones to further delineate that space between the street and the building.

Commissioner Alcorn: I appreciate your comments on it because I think that shows you can do it, but if you start with a straight road it's a lot easier to put a building in an urban framework like this. So where we don't have roads yet, I know that's one of the things that we're working on is straight roads. And this is a great example of – sometimes you can make it work, but boy if were straight it would be a lot easier. Thank you.

Mr. Krieger: We'd be happy to help you with that.

Commissioner Alcorn: I bet you would. Thank you.

Chairman Murphy: All right. Mr. Lawrence.

Commissioner Lawrence: I think you see in Commissioner Alcorn a man with an agenda and I must say that I subscribe to that agenda. And I can't resist a comment. Tysons is fascinating because we have here a city in its birth that was born in the age of cars. Look at the cities we have now. They were not so born. So buildings rise up out of the ground and streets were not much of a consideration other than shank's mare and deliver wagons when a lot of these cities were laid out. That's not the case in the new Tysons. So between that and topography and some other things, we're seeing a lot of podium designs. And here we have an example of one. There are many others so for those who happen to be watching this, stay tuned. It's very interesting. Thank you very much for your help with these questions. I don't hear any more and I think that concludes the questions that we had.

Chairman Murphy: Right.

Commissioner Lawrence: Mr. Chairman, I'm ready to move for a vote.

Chairman Murphy: Please.

Commissioner Lawrence: Mr. Chairman, I MOVE THAT THE PLANNING COMMISSION FIND ITSELF IN SUPPORT OF THE ARCHITECTURE PRESENTED IN THE PACKAGE DATED NOVEMBER 8TH, 2011, FOR THE MACERICH PHASE 1 OFFICE BUILDING, THE GARAGE F1 FAÇADE, AND THE LANDSCAPING ALONG ROUTE 123.

Commissioner Alcorn: Second.

Chairman Murphy: Seconded by Mr. Alcorn. Is there a discussion of the motion? All those in favor of the motion as articulated by Mr. Lawrence, say aye.

Commissioners: Aye.

Architectural Review of Portions Of Phase 1 of Tysons Corner Center (Macerich)

Chairman Murphy: Opposed? Motion carries. The Chair abstains, I was not present for the public hearing that evening, but if I were here I would have voted for it [sic].

Commissioner Alcorn: There wasn't public hearing. This is administrative.

Commissioner Lawrence: Yes, this is just an administrative item.

Chairman Murphy: Oh, this is an administrative item. I'm sorry.

Commissioner Alcorn: So you were right. You weren't here.

Commissioner Lawrence: The public hearing was a long time ago.

Commissioner de la Fe: Don't you remember? Back in 2004?

Chairman Murphy: Oh, oh. Where have you been all these years? I didn't see you, Dave.

Commissioner Alcorn: Long deferral.

Chairman Murphy: I thought it was part of the thing I missed. Okay.

Commissioner Alcorn: It is Providence.

Chairman Murphy: I'll retract that. I'll vote for it.

Commissioner Lawrence: I look forward to more administrative reviews like this, and thank you for the package that gives us the information we need. Thank you very much.

Mr. Krieger: Thank you.

//

(The motion carried unanimously with Commissioners Hall and Harsel absent from the meeting.)

JLC

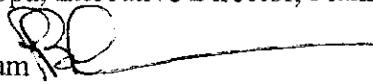


County of Fairfax, Virginia

MEMORANDUM

DATE: December 1, 2011

TO: Members, Planning Commission
Barbara Lipka, Executive Director, Planning Commission

FROM: Peter Braham 
Zoning Evaluation Division, DPZ

SUBJECT: Architectural Review of Portions of Phase 1 of the Tysons Corner Center

Reason for Planning Commission Action:

On January 22, 2007, the Board of Supervisors approved RZ 2004-PR-044, rezoning the Tysons Corner Center to the Planned Development – Commercial (PDC) District subject to proffers dated January 22, 2007. Included in the proffers were commitments contained in Proffers 17, 18 and 20 that require Planning Commission review, among other things, of:

- 1) Building architecture and materials of the buildings within Phases 1 and 2;
- 2) The parapet walls and the penthouses, to include screening of the elevators and mechanical equipment and the appearance of the screening, including the penthouses, of all the buildings; and,
- 3) The design, materials and the elevations of the wall along Route 123 that also serves as the podium for the office building and the residential building that are part of Phase 1.

The applicant has submitted a package containing illustrative elevations of the office building in Phase 1 and the wall facing Route 123 for the review and approval by the Planning Commission. The package is entitled Tysons Corner Center Phase 1: Office Architecture & Route 123 Wall/Landscaping and is dated November 8, 2011. Copies of the package containing 11x17 illustrations and text along with a cover letter were delivered to the commission by the property owner. The package includes the applicable text of the proffers.

The proffers specify that Planning Commission approval is required prior to the issuance of building permits for the features affected by these three proffers, which include the Phase 1 office building and the retaining wall along Route 123. It should be noted that the applicant will be presenting the architecture of the other two buildings within Phase 1, the hotel and the residential building; the pedestrian bridge between Phases 1 and 2; the wall along Rt. 123 in Phase 2 and the buildings within Phase 2 for Planning Commission review at a later time. As also required by Proffer Number 20, the Commission will have an opportunity to review and

comment on the compatibility of the Phase 2 wall that will also be located along Rt. 123 at the South Entrance Pavilion and the bus plaza located west of the entrance to Tysons Corner Center with the wall within Phase 1, when those plans are prepared.

Background:

The package submitted by the applicant includes a sheet depicting the layout of the redevelopment of the Tysons Corner Center as a transit-oriented project. This layout reflects the one shown on the proffered CDP/FDP for RZ 2004-PR-044 approved by the Board of Supervisors on January 22, 2007. Phase 1 is generally located on the site of the former Circuit City and associated smaller stores with a portion to be constructed across the ring road through the mall property. The Circuit City and other stores have been demolished.

The property owner (Macerich) has submitted several proffer interpretation requests with regard to Phase 1. These have included, among other things, the following minor modifications found in substantial conformance with the proffers and the CDP/FDP regarding:

- A revised layout for the plaza at the future entrance to the second level of the mall and between the Lord & Taylor building and the approved hotel;
- A shift of forty multi-family dwelling units from the hotel building to the residential building;
- A revised footprint for the office building, including an increase in the amount of GFA within the building devoted to retail uses and the extension of the lobby to two floors, one at the plaza level and one at the level of the ring road (this is illustrated in the package before the Commission);
- An increase in the number of stories in the office building, reflecting a smaller footprint, while still adhering to the specified building height, 320 feet, and amount of GFA, 551,638 sq. ft.;
- An increase in the size of the plaza located along the Rt. 123 and Beltway ramp ROW that was the result of the relocation of a Fairfax Water main and the need for less right-of-way; and,
- The relocation of the water main also facilitated the redesign of the loop trail through this portion of Tysons Corner Center allowing it to be straightened while still meeting the existing grade at the intersection of Westpark Drive and the ring road, which is established by the bridge landing;
- The previously noted shift in the right-of-way allowed for the elimination of two retaining walls, thereby placing the landscaping closer to the level of the traffic along the adjacent roadways, and permitting the façade of the parking garage podium to be the face of the project in this area rather than two retaining walls, one of which would have been built to VDOT specifications.

Another interpretation, concerning Phase 2, addresses an increase in the amount of right-of-way dedicated for the Metrorail project and a new design of the South Pavilion, the entrance to the future Tysons I and Tysons II station, to reflect a determination by the Federal Transit

Administration that the South Pavilion was required by law to be owned and operated by WMATA rather than the owner of Tysons Corner Center as was contemplated at the time of the rezoning approval. Dulles Transit Partners will be following through on Planning Commission review of the South Pavilion as reflected in the proffers for Tysons Corner Center.

Discussion:

Office Building Architecture

The illustrations and commentary provided by the property owner, Macerich, describes the office building as one that is clad in glass, aluminum and architectural concrete. The building has been designed in a manner that provides banding that identifies individual floors. In addition, the building includes elements that divide the building façade into several different planes and includes vertical banding that further divides up the building façade. As noted in the text, the office building includes the top or roof level that is clad in glass and topped by pin lights. This level includes all of the elevator housing and the mechanical equipment associated with the building. The top of the exterior or parapet wall and a majority of the penthouse structures are located within the proffered 320 foot height limitation, and; therefore, are not extensions above the allowed building height.

Proffer Number 17 states that the Planning Commission is to review "...whether the building heights, building articulation and other architectural design characteristics of the proposed buildings are in furtherance of the objectives of the County's Comprehensive Plan for the Tysons Corner skyline."

The Urban Design Guidelines of the Tysons Corner Urban Center address building height by setting a gradient of building heights that descends as the distance from the future locations of the Metrorail stations increases. The building under review is located within first tier, where heights of 225 to 400 feet are recommended. This 320 foot tall building is consistent with this recommendation. The proposed design also responds to the applicable standards included on page 115 of the Plan text under the heading "Building Heights" as follows:

- The measured building height includes the parking structure, which is part of the podium on which this building and the residential tower building in this portion of Phase 1 sit. (The architectural character of the wall along Route 123 is addressed below).
- The rooftop mechanical equipment is hidden from view from the street level by the rooftop or parapet wall.
- The building is situated so as to take advantage of views to the north and east.
- The design of the building is iconic, as defined in the Plan text, in that it is well crafted by being of quality materials and appears to be an office building, thereby reflecting its use. Further, the building incorporates elements that establish a defined first level, architectural elements that define each floor and the longer glass panels to cap the structure. In addition, the façade includes offsets and panels which will distinguish this building from its neighbors.

Parapet Wall and Screening of Mechanical Equipment

The wall that screens the elevator penthouses and the mechanical equipment is an integral part of the building and all those features are located below the maximum proffered height limitation of 320 feet. This ensures that the mechanical equipment and elevator penthouses are similar in material and color to the building and are integrated into the overall building, as addressed by Proffer Number 18.

Wall along Route 123

Pursuant to the previous interpretation issued on August 25, 2009 and reaffirmed in one dated November 18, 2011, several minor modifications were approved relating to the face of the parking garage podium along Rt. 123. Whereas the proffered CDP/FDP showed a retaining wall, several changes resulting from engineering refinements to the Capital Beltway HOT Lanes project, have resulted in allowed design changes to the shape and extent of the podium. The engineering changes included the relocation of a waterline easement and a reduction in the amount of right-of-way required, thereby allowing the footprint of the garage to expand and be more regular in shape, thereby widening the plaza on top of the podium. The revised garage footprint is no closer to the right-of-way line than 8 feet, which was the smallest setback shown on the proffered CDP/FDP. In addition, engineering coordination has allowed for the elimination of two retaining walls – one within the bounds of the road project and the one along the podium shown on the proffered plans. With the elimination of the retaining walls, the northern face of the garage in effect becomes the wall addressed by Proffer Number 20 and that is the area for consideration by the Commission. These changes have resulted in an expanded area for landscaping, an area that is similar in elevation to the roadway, rather than perched above the road behind a retaining wall. While the height of the façade presented to Rt. 123 varies, in portions it has also grown to reflect the lowered grade.

The northern façade of the garage along Rt. 123 in Phase 1 will be a prominent feature as one enters Tysons Corner along Route 123 from McLean and from the interchange of Route 123 with the Capital Beltway. It will also be visible from the tracks of the future Silver Line. As noted above, this façade will be part of the base or podium upon which the planned residential building and the office building addressed in this memorandum will sit. Pages 19 through 22 of the package submitted for review by the Planning Commission include an elevation view and an axiomatic view of the wall respectively. These illustrations include the proposed landscaping at ground level between the roadway (at approximate elevation of 420 ft.) and the wall and the plantings on top of the podium or the plaza which is adjacent to both buildings on top of the podium. The plantings shown on the plaza occur at two levels, the lower level (at approximate elevation 445 ft.) which provides a fire lane connection on either end of the podium to the ring road around the mall property. The upper level of the plaza (at approximate elevation 469 ft.) occurs at the level of the first floor of each of the two buildings. The upper plaza occurs at the same level as the plaza connecting to the mall and the bridge between Phase 1 and the South Entrance Pavilion for the future Metrorail station.

As shown on the illustrations, the trees at the level of Rt. 123 will provide a varied screen of the wall consisting of individual masses of evergreen, flowering ornamental trees and other shade trees. Shrubs and ornamental grasses are proposed for additional visual interest at ground level. These plantings are to be native species. The major portion of the wall visible from the roadway will consist of green screening (lattice-like screening with planted evergreen and deciduous vines running up the lattice work) that is capped by a grey stone veneer wall. There are two buff colored fieldstone panels that extend the full height of the wall; one located at the westernmost end of the wall and the second one located in the area that coincides with the gap between the two buildings. These features combine to "...lessen the apparent mass of the wall..." as specified by Proffer Number 20.

Conclusion

This memorandum is submitted for the consideration of the Planning Commission as part of their review and approval of the architecture of Parking Garage F-1 and the Phase 1 office building depicted in the package of materials submitted by Macerich pursuant to the proffers for RZ 2004-PR-044.



RECEIVED
Department of Planning & Zoning
NOV 23 2011
Zoning Evaluation Division

November 22, 2011

Fairfax County Planning Commission
12000 Government Center Parkway, Suite 330
Fairfax County, Virginia 22035-0071

RE: RZ 2004-PR-044 Proffer Requirements for Planning Commission Review of the Architecture, Parking Terrace Façade and Landscaping (Providence District)

Dear Commissioner Murphy, Commissioner Lawrence and Members of the Commission:

I hope this letter finds you well. Enclosed please find a package of materials for your review and consideration at the December 8, 2011 Planning Commission meeting.

As noted on Sheets 5 and 17, proffers approved in 2007 with RZ 2004-PR-044 (and a subsequent interpretation) require Macerich to seek review and approval of the building architecture, the façade treatment of the Phase 1 parking terrace (F-1) and the landscaping along Route 123. Prior to review by the Commission, consultation on each of these items with the Providence District Supervisor is required.

This package of materials includes information on the Phase 1 office building architecture, Parking Terrace F-1 façade and the landscaping along Route 123. Each of these items has already been reviewed with the Providence Supervisor, and Supervisor Smyth has indicated her support of the plans as enclosed in this package. As required by the proffers, at such time as the architecture information is available for the Phase 1 residential and hotel towers and the pedestrian bridge, Macerich will return to the Commission for review of these additional items.

Thank you for your assistance with the review of these materials. Please feel free to contact me with any questions at 703-798-9560.

Sincerely,

Hillary Katherine Zahm, AICP
Senior Manager, Development
Macerich

cc: Supervisor Linda Q. Smyth, Providence District
Peter Braham, Department of Planning and Zoning
John Harrison, Macerich
David Piper, Macerich

Enclosures



Gensler

TYSONS CORNER CENTER PHASE 1:
OFFICE ARCHITECTURE & ROUTE 123 WALL / LANDSCAPING

TCC Office Planning Commission Presentation

PHASE 2
PROPOSED GROSS FLOOR AREA:

OFFICE	488,290± SF
COMMERCIAL	48,500± SF
RESIDENTIAL	518,000± SF
(408 UNITS MAX.)	
SOUTH ENTRANCE PAVILION	13,000± SF
TOTAL	1,067,790 SF

PHASE 1
PROPOSED GROSS FLOOR AREA:

OFFICE	551,638± SF
COMMERCIAL	25,274± SF
HOTEL	266,513± SF
RESIDENTIAL	542,175± SF
(395 UNITS MAX.)	
TOTAL	1,385,600 SF

PHASE 4
PROPOSED GROSS FLOOR AREA:

OFFICE	281,300± SF
COMMERCIAL	68,100± SF
RESIDENTIAL	320,000± SF*
(300 UNITS MAX.)	
TOTAL	669,400 SF

TOTAL EXISTING AND PROPOSED GROSS FLOOR AREAS:

EXISTING MALL	2,399,135± SF
PLANNED MACY'S EXP.	75,000± SF
PROPOSED OFFICE	1,347,328± SF
PROPOSED HOTEL	266,513± SF
PROPOSED COMMERCIAL	197,674± SF
PROPOSED RESIDENTIAL	1,728,175± SF**
(1,385 UNITS MAX.)	
PROPOSED SOUTH ENTRANCE PAVILION	13,000± SF
TOTAL	6,026,825 SF

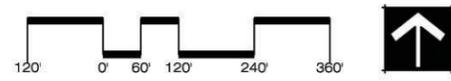
PHASE 3
PROPOSED GROSS FLOOR AREA:

OFFICE	26,100± SF
COMMERCIAL	55,800± SF
RESIDENTIAL	298,000± SF*
(282 UNITS MAX.)	
TOTAL	379,900 SF

* NOTE: IN ADDITION TO THE RESIDENTIAL SQUARE FOOTAGE SHOWN ABOVE, AN ADDITIONAL TOTAL OF 50,000 S.F. OF RESIDENTIAL DEVELOPMENT CAN BE ALLOCATED TO EITHER PHASE 3, PHASE 4, OR BETWEEN BOTH PHASES.

* NOTE: IN ADDITION TO THE RESIDENTIAL SQUARE FOOTAGE SHOWN ABOVE, AN ADDITIONAL TOTAL OF 50,000 S.F. OF RESIDENTIAL DEVELOPMENT CAN BE ALLOCATED TO EITHER PHASE 3, PHASE 4, OR BETWEEN BOTH PHASES.

- NOTES:
- PER PROFFER #6, A CDPA AND FDPA SHALL BE APPROVED BY THE BOARD OF SUPERVISORS AND PLANNING COMMISSION, RESPECTIVELY, PRIOR TO THE APPROVAL OF A SITE PLAN FOR ANY RESIDENTIAL BUILDINGS, OFFICE BUILDINGS OR PLAZAS BEYOND PHASE 2 (INCLUDING BUT NOT LIMITED TO THESE FEATURES INDICATED IN PHASES 3 AND 4 IN THIS CDP/FDP).
 - THE NUMBER OF STORIES, FOOTPRINT AND SQUARE FOOTAGE OF THE SOUTH ENTRANCE PAVILION IS SUBJECT TO CHANGE DEPENDING ON THE FINAL DESIGN OF THE METRO PEDESTRIAN CONNECTION TO THE ENTRANCE PAVILION.



Patton Harris Rust & Associates, p.c.
Engineers, Surveyors, Planners, Landscape Architects

14330 Lee Road, Suite 101-1079
T 703.448.6700
F 703.448.6714

PHRA

RTKL
Associates, Inc.
RTKL Pacific Avenue
Dallas, TX 75201
T 214.871.7023

PHASE-USE PLAN

PROJECT
CDP / FDP
TYSONS CORNER CENTER
RZ 2004-PR-044
PROVIDENCE DISTRICT
FAIRFAX COUNTY, VIRGINIA

DESIGN	PHR+A	DATE	NOVEMBER 15, 2004
RAWN	SME	REVISED	JAN. 08, 2007
CHECKED	DHS	SCALE	1" = 120'
HEET	PH04PHASE-USE	FILE NO.	04256-G-5
			4 OF 63

S:\14256-G-5\Planning\PH04PHASE-USE.dwg, Phase 0404, 1/16/2007 1:56:57 PM, Created by: sdc, MCH 10, 111

OFFICE ARCHITECTURE

Tysons Tower Office Building

Architecture. The proffers associated with RZ 2004-PR-044 (the “Rezoning”) require that buildings in the four phases of development at Tysons Corner Center be designed with high quality architecture and building materials such as glass, steel, brick, masonry, architectural pre-cast, stone masonry, architectural concrete and/or other similar materials typical of Class A office buildings. The Phase 1 office tower (Building 1-B) at Tysons Corner Center has been designed with a strong focus on aesthetics. Exterior building materials consist of glass, aluminum, architectural concrete and materials of high-quality and finish that would typically be found on a Class A or Trophy office building. The podium base of the tower consists of stone masonry, architectural precast, glass and architectural metal. As required by the proffers, no Exterior Insulation and Finish Systems (EIFS) will be utilized on the office tower.

The office tower has carefully designed floor plates that promote extraordinary views and universal access to natural light. Great panoramic views of the surrounding areas are coupled with quality glazing that allows natural daylight to permeate deep into the workspace, facilitating employee health and well-being as well as reducing the dependence on active lighting fixtures.

Animated Façade Areas. Subsequent to approval of the Rezoning, the Department of Planning and Zoning issued a proffer interpretation that permitted retail/restaurant uses at the plaza and ground floor levels of the Phase 1 office building. With these uses, the County required that the retail/restaurant spaces be designed in conformance with the proffered guidance for Animated Façade Areas. While the retail spaces will not be fully designed until tenants are in place, efforts have been made to ensure that Animated Façade Areas will be provided.

- At the plaza level all the spaces have transparent and open glazing which allow views to the tenant space. At the small retail area at the street level 48% of the street frontage is clear glazing.
- At the plaza level, the areas outside the retail space will include plaza landscaping and a seating terrace, canopied by the upper floors of the office building. The retail facades at the plaza levels are completely transparent glass so the activity and animation of the restaurant engage the plaza and street.
- The retail spaces at both the plaza and the street level have consistently spaced mullions to allow ultimate flexibility to prospective tenants as to their entry locations.

The street level retail is provided with lush tree canopies and an adjacent seating area at the 2 story expansive lobby which visually connects plaza and street levels and adds another level of activity to the site.

Building Height & Penthouse

The current Fairfax County Comprehensive Plan for Tysons Corner recommends that the tallest buildings be located within 1/8 mile of the Tysons Metro stations. The Rezoning for Tysons Corner Center (RZ 2004-PR-044) reflects this guidance, with the tallest proposed buildings to be located in Phases 1 and 2. This iconic office tower is located within Tysons Corner Center’s approved Phase 1 development and will meet the County’s height limitations. This trophy design and quality building, along with the other towers on the Tysons property and on neighboring properties, will help to enhance the Tysons Corner skyline with a variety of building designs and heights.

The CDP/FDP, and subsequent proffer interpretations, restrict the office building to 320’ in height and 14-20 stories. The proffers restrict the penthouse height beyond the 320’ office height to 25’ unless additional height is needed to accommodate the elevator. Macerich proposes to provide a 20-story office building. The height of the office building, including the penthouse and rooftop equipment, will not exceed 320’. Because the entire building (including the penthouse) is below the 320’ height maximum, the proffer provision regarding penthouse height is not applicable. The height of the office tower is measured from the average grade of the parking podium as discussed and confirmed with County Staff with the Department of Planning and Zoning.

The proffers require that the penthouse be similar in material and color to the building, so the penthouse appears to be integrated with the building. The proposed predominantly glass building and penthouse are seamless in appearance. Macerich gave great consideration to the building design to ensure that the roof was not an afterthought but that the building and penthouse will be integrated to provide an impressive, striking structure from top to bottom. The penthouse façade is comprised of glass

As required by the proffers, mechanical equipment located on the roof is screened from ground level view. Devices are setback behind an opaque screen wall that is located 11’ from the glass parapet.

BUILDING ARCHITECTURE & TELECOMMUNICATIONS EQUIPMENT

17. Architecture. Buildings shall be designed with high quality architecture and building materials. The exterior building materials used in the development of the new residential, office and hotel buildings and the South Entrance Pavilion shall consist of glass, steel, brick masonry, architectural pre-cast, stone masonry, architectural concrete and/or other materials of similar quality that are typically used on the exterior of Class A office buildings and residential and hotel buildings of a similar quality. No Exterior Insulation and Finish Systems (EIFS) shall be utilized on any of the new proposed residential, office or hotel buildings or the South Entrance Pavilion. At or prior to the time of submission of building plans to Fairfax County, the Applicant shall provide information on (i) the building architecture and materials (including the materials and design of the pedestrian bridge between Phases 1 and 2), (ii) the parapet walls or screening walls as specified in Proffer #18 and (iii) the animated façade areas as specified in Proffer #19 to the Planning Commission for review and comment after consultation with the Providence District Supervisor; no such building plans shall be approved by the County until the Planning Commission indicates support for the architecture. In the course of its architectural review, the Planning Commission shall consider, among other things, whether the building heights, building articulation and other architectural design characteristics of the proposed buildings are in furtherance of the objectives of the County's Comprehensive Plan in enhancing the Tysons Corner skyline.

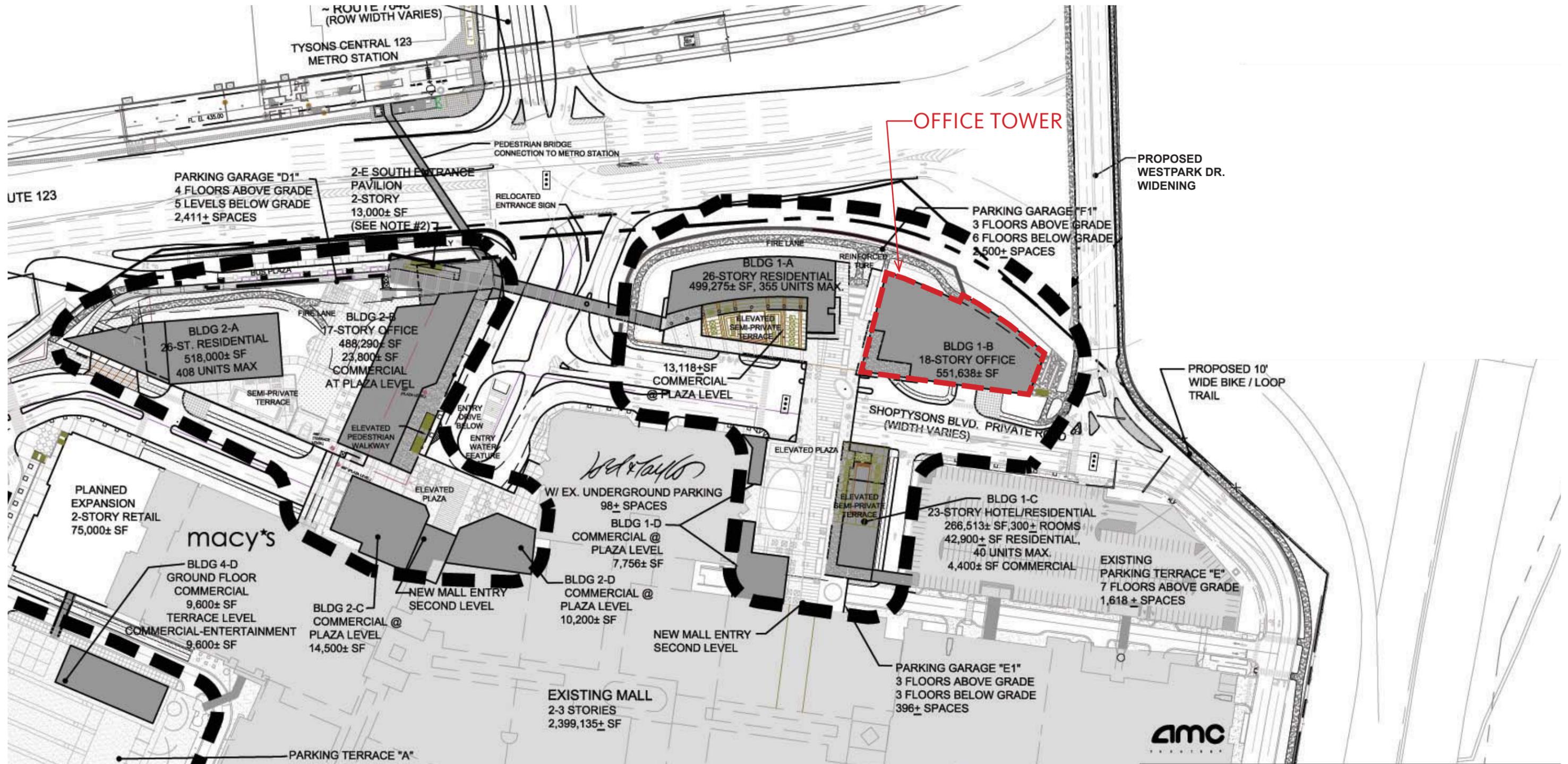
18. Maximum Building Heights. The maximum heights of the proposed buildings shall not exceed the building heights indicated on the CDP/FDP. As indicated on the CDP/FDP, the Applicant is committed to maximum building heights and to a range of number of stories. This height limit does not include penthouses, elevators or mechanical equipment rooms covering less than 25% of the roof pursuant to Section 2-506 of the Zoning Ordinance. Penthouses shall be similar in material and color to the building, so the penthouse structure is consistent with the rest of the architecture of the building and is architecturally integrated with the building. No such penthouse structure shall exceed 25 feet in height, unless it can be demonstrated by the Applicant that such additional height is needed for the elevator the Applicant selects for that building; in which case, the maximum height of the penthouse structure shall not exceed 30 feet. The Applicant shall screen mechanical equipment located on the rooftops of the proposed buildings from ground level view, using opaque parapet walls and other screening walls, materials or devices.

19. Animated Façade Areas. The Applicant shall provide animated façade areas throughout the Property as identified on the CDP/FDP and which shall be indicated on the architectural/building plans submitted per Proffer #17.

A. External Streetscape Presence. The Applicant shall require new tenants located in the areas identified in the CDP/FDP as "Animated Façade Areas" to create an external streetscape presence through the use of transparent exterior storefront facades (as outlined below in paragraph C) and entries, landscaping, seating areas, canopy and awning shade elements and other techniques that create a building façade that provides interest to pedestrians and vehicles. When negotiating new leases with existing tenants located in the Animated Façade Areas, the Applicant shall require existing tenants to also create such external streetscape presence using the foregoing techniques.

B. Entry Elements. The Animated Façade Areas shall include functioning entry doors into the applicable Retail/Commercial space. Such entry elements shall not be separated by a distance of more than 75-feet on-center, unless a greater separation is needed to accommodate larger tenant spaces or as permitted by the Zoning Administrator. Should the layout of a larger tenant not be able to accommodate multiple entries with a maximum spacing of 75 feet, such tenant shall be required to design the façade with glazed elements that are no more than 25-feet apart, and of a size no smaller in area than 48 square feet.

C. Glazing Requirements and Transparency Levels. Along the Animated Façade Areas, a minimum transparency level of 45% of the overall area of the façade (as measured across the entire length of the façade and to a height of 15-feet from the lowest finished floor elevation) shall be provided through glazed windows and doors that allow views into the tenant space. Up to 20% of the above described transparent windows or glazing can be met with display windows or glazing that have reduced visibility through sandblasting, glass blocks or other similar methods, that allow for light to enter the interior space without providing direct visibility from outdoors, with the remaining glazing remaining fully transparent.





Parking Podium façade for illustrative purposes only. Podium façade treatment provided with Route 123 Wall and Landscaping graphics later in this package

View from Northeast





Streetscape and tree locations shown for illustrative purposes only.

View from Southeast





View from Southwest



Streetscape and tree locations shown for illustrative purposes only.

Tyson's One Place (Formerly Shop Tyson's Boulevard) Approach View





ENTRY LOBBY AT STREET AND PLAZA LEVELS - 100% CLEAR GLASS

PLAZA LEVEL RETAIL - 100% CLEAR GLASS

COVERED CANOPY

RETAIL TERRACE SEATING. SEE PLAN

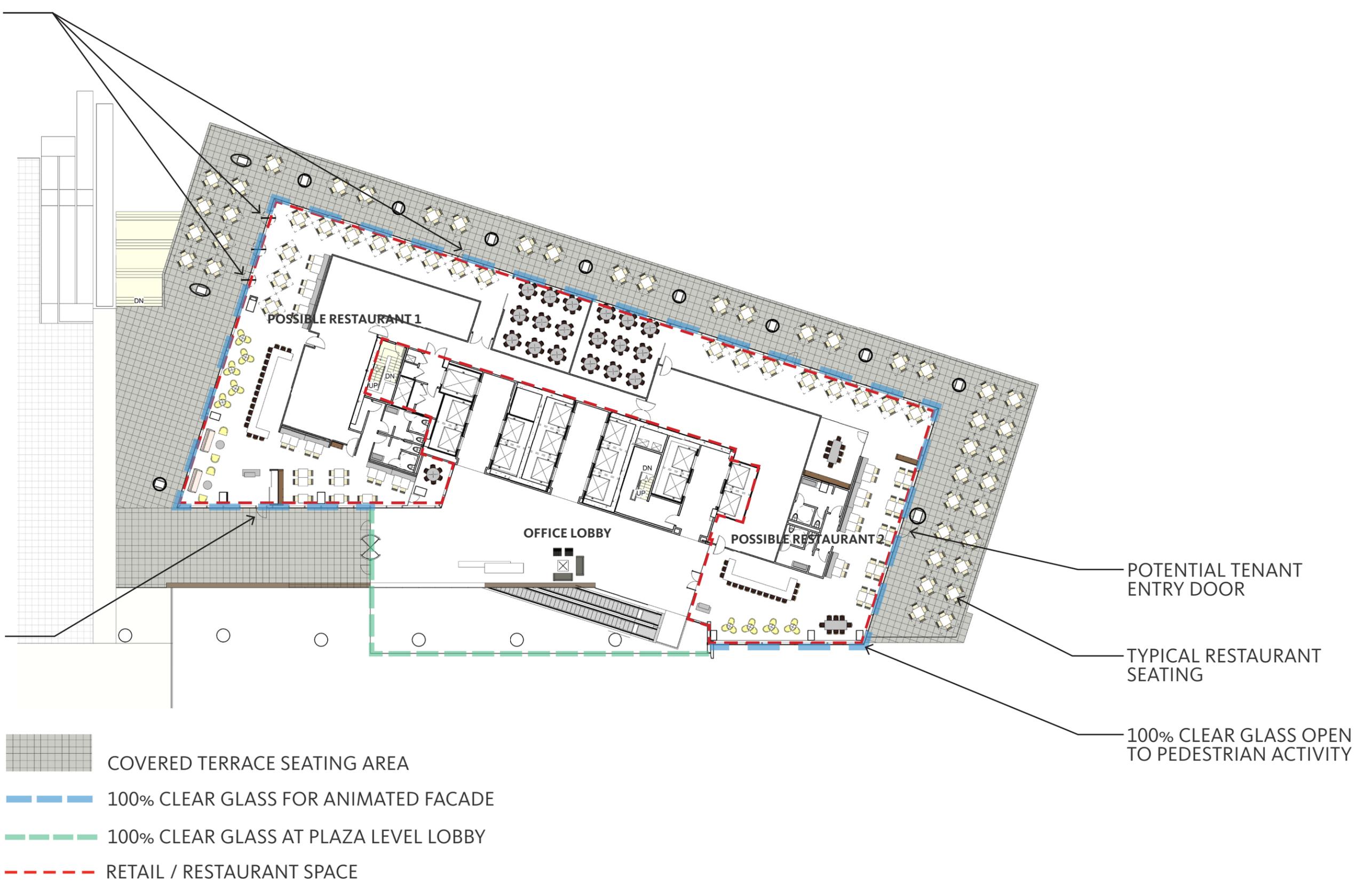
BENCH SEATING. SEE PLAN

STREET LEVEL RETAIL - 48% CLEAR GLASS

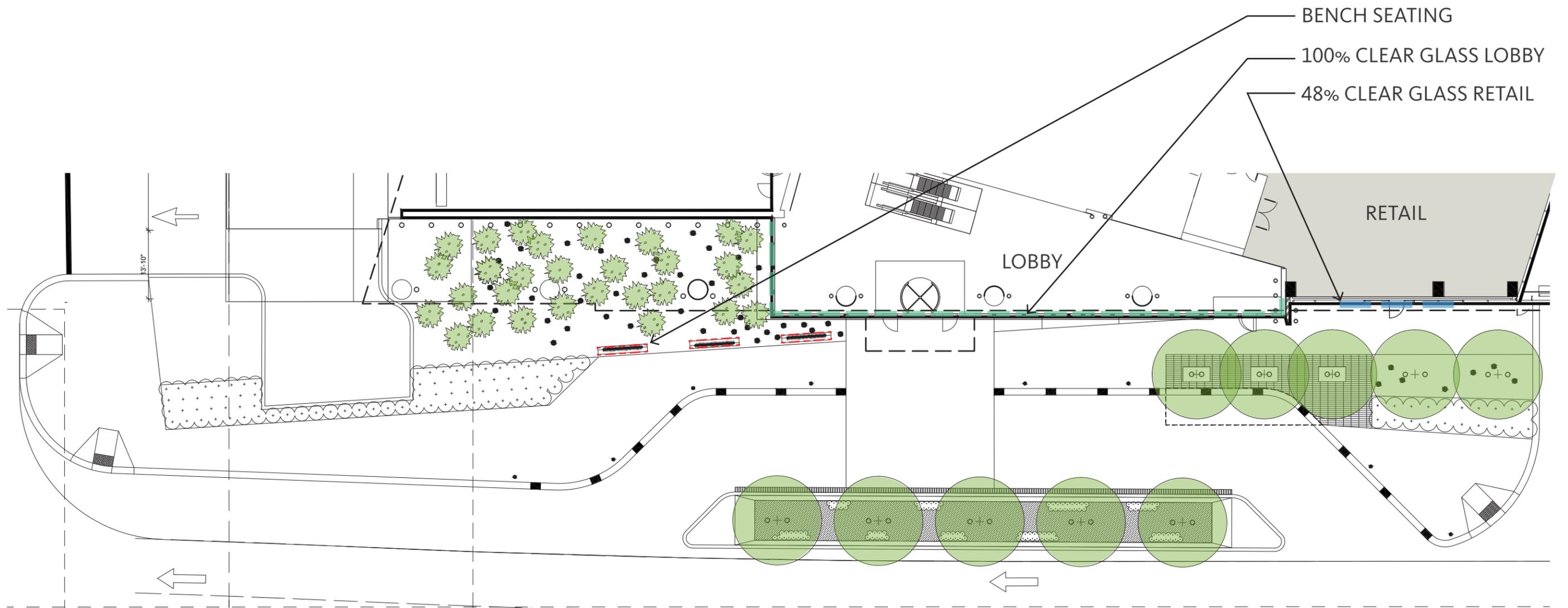
SouthEast Rendering

POTENTIAL TENANT ENTRY DOORS

POTENTIAL TENANT ENTRY DOOR



Plaza Level Plan



BENCH SEATING
 100% CLEAR GLASS LOBBY
 48% CLEAR GLASS RETAIL

LOBBY

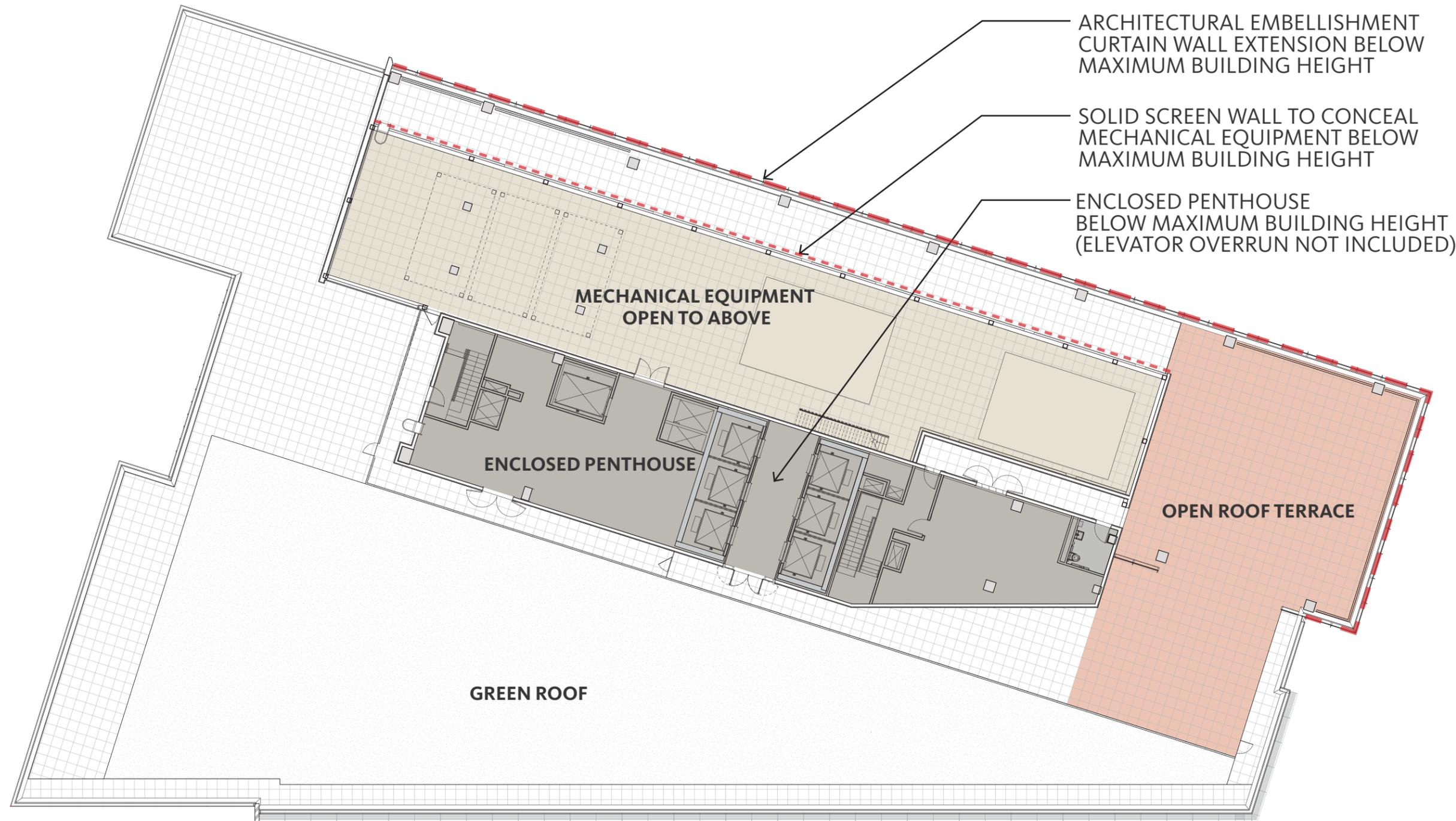
RETAIL

-  COVERED TERRACE SEATING AREA
-  100% CLEAR GLASS FOR ANIMATED FACADE
-  100% CLEAR GLASS AT PLAZA LEVEL LOBBY
-  PLANTED LANDSCAPE
-  BENCH SEATING

Street Level Entry



PENTHOUSE AND ARCHITECTURAL FEATURES BELOW MAXIMUM BUILDING HEIGHT. MAXIMUM BUILDING HEIGHT 320' MEASURED FROM AVERAGE GRADE.



Illustrated Roof Plan

ROUTE 123 WALL AND LANDSCAPING

Tysons Tower Office Building Landscape

Route 123 Podium Facade

An attractive, impressive view of the Phase 1 structures and parking podium from Route 123 and the future elevated Metro Silver Line is critical to Macerich and Tysons Corner Center. Macerich proposes a high-quality, attractive façade utilizing fieldstone: a material and color that will complement the Phase 1 office and residential designs.

The office design indicated in the preceding sheets of this graphics package indicates that the colors of the office building and parking podium are consistent and compatible. The residential tower, which is not yet designed, will also be compatible with the parking podium façade as well as the adjacent office building.

The mass of the parking podium façade is broken up with the wall design as well as the landscaping further described below. The wall includes a mixture of high quality materials: fieldstone, limestone, and greenscreen to provide visual interest, a “green” look and texture and to minimize the appearance of the façade itself. Materials of differing color provide accents and variety to the linear wall, and the greenscreen complemented by the landscaping along Route 123 allows the base of the Phase 1 office and residential towers to appear very “green” and less structural than a typical building base.

Macerich has reviewed the parking podium wall and landscape design with Supervisor Linda Smyth and Todd Nelson, Fairfax County Urban Forester.

Route 123 Frontage Landscaping

Landscaping is an important component of the future development of the shopping center and the new phases. During the Rezoning, the County placed particular emphasis on the treatment of the landscaping between Phase 1 and Route 123. Landscaping along the Phase 1 Route 123 frontage includes planting materials above the parking garage on the podium/ lower plaza at the north side of the Tower and at grade north and east of the garage adjacent to Rt. 123 and the ramp on to I-495. As discussed above, to supplement the planting at-grade and on the plaza, a greenscreen wall is proposed along the Phase 1 parking terrace façade. The plant materials indicated in the following graphics reflect a 10-year canopy.

As required by the proffers, deciduous and evergreen trees and shrubs are proposed between the Phase 1 parking terrace façade and Route 123. Shade trees will be installed with a minimum 4” caliper, and evergreens will be a minimum of 8’-10’ in height at the time of installation. As noted above, the landscaping plan has already been reviewed by Todd Nelson, Urban Forest Management Division, DPWES, and revisions based on his comments were incorporated into the landscape plan.

1. Podium/Lower Plaza

The predominant plant materials on the podium/lower plaza area are ground cover plants and ornamental grasses planted on grade and in low planters designed to look like a tapestry of various textures and colors. The variety of materials will provide visual significance to viewers from the office towers above as well as visitors to the site. Ornamental trees are located in key areas including a row on either side of the bike trail leading into the site. To accommodate a County-required fire lane running through the site, a combination of concrete pavers and Grasspave has been creatively utilized to provide a drive-able path while safely “blurring the lines” a bit for visual interest. Low planter walls at the east end of the plaza provide a visual cue of the terminus of the fire lane while framing pathways to an overlook.

2. At-Grade

This area is not on-structure and is intended to be a native planted landscape on sloping terrain. Trees include Oaks, Black Gum, Southern Magnolias, Eastern Redbud and Red Cedars. A small grouping of Metasequoia is located to serve as a focal point. The slopes are planted with ornamental grasses, and amidst the grasses is a drift of Viburnum, Hydrangea and Wax Myrtle shrubs. A cluster of Cedar trees, Japanese Cryptomeria, Norway Spruce and Sumac frame the trail on the east side of the building adjacent to the Westpark Bridge. The North garage wall will contain a greenscreen with a matrix of deciduous and evergreen vines which will “break up” the massing of the North podium wall and provide visual interest to those traveling along Route 123.

20. Retaining Walls Along Route 123. The Applicant shall design the walls along Route 123 to provide high-quality attractive facades. The primary material for the façade of the walls shall be high-quality architectural block, stone, stone-like material, colored pre-cast concrete or a comparable material. At the time of submission of site plans to Fairfax County for Phases 1 and 2, and after the Applicant has consulted with the Providence District Supervisor, the Applicant shall provide the proposed design, building materials and elevations of this wall to the Planning Commission for review and comment. Site Plan approval shall not be granted for either the Phase 1 or Phase 2 residential and office buildings without a positive recommendation about the wall from the Planning Commission.

A. Phase 1 Wall. The wall associated with Buildings 1-A and 1-B along Route 123 shall be no greater than 34' in height at any point (including the up to 5' sound attenuation wall), as shown on the CDP/FDP. The Planning Commission shall be permitted to review and comment on the quality of the proposed wall, the compatibility of the wall with the architecture proposed for Buildings 1-A and 1-B and with the rest of the site, the use of appropriate landscaping, and the relationship between the wall and the screening and plantings. In furtherance of the foregoing, the Planning Commission's review shall include the following elements:

- i. The Applicant shall integrate landscape screening and treatment of the wall to lessen the apparent mass of the wall. The Applicant shall incorporate architectural elements or themes from Buildings 1-A and 1-B into the façade of the wall to provide a high-quality, attractive street-level façade along Route 123.
- ii. The wall shall be articulated through changes of plane, lighting, material, color or other architectural means to lessen the apparent mass of the wall. Consideration shall be given to the use of horizontal features that serve to reduce the apparent height of the wall.
- iii. Landscaping, including deciduous and evergreen trees and shrubs, shall be utilized between the wall and Route 123 to provide visual interest. Shade trees shall be a minimum four-inch (4") caliper, and evergreen trees shall be a minimum of 8 to 10 feet in height at the time of installation. Final determination of the tree species and specific planting locations shall be approved by Urban Forest Management at the time of site plan review.
- iv. Sheet 15 of the CDP/FDP includes images of walls that exhibit possible wall materials as well as the quality and visual interest proposed for this wall.

B. Phase 2 Wall. The Planning Commission shall be permitted to review and comment on the proposed Phase 2 wall to verify its compatibility with the Phase 1 wall, through the use of similar primary materials, architectural elements, colors and/or other elements.



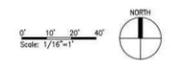
MATERIALS

- 1 SCORED CONCRETE
- 2 GRANITE CURB - FLUSH W/PAVE
- 3 GRANITE CURB - RAISED
- 4 GRANITE PAVER
- 5 3-D DIAMOND ASPHALT PAVER
- 6 CONCRETE PAVER
- 7 STEEL GRATE OVER CRUSHED GRANITE
- 8 RIVER COBBLE
- 9 POURED IN PLACE CONCRETE
- 10 FLUSH STEEL EDGE
- 11 GRANITE BOLLARD
- 12 STAINLESS STEEL PLANTER WALL
- 13 STAINLESS STEEL BENCH
- 14 CURB RAMP
- 15 ROOT PATH
- 16 OUTDOOR TABLE & CHAIRS

PLANTS / Quantities for Trees & Shrubs

- 17 SEDGE - 58
- 18 NUDE SHEATH BAMBOO - 65
- 19 NUTTAL OAK - 10
- 20 GINKGO BILOBA - 19
- 21 FRINGETREE - 57
- 22 TIGER EYES SUMAC - 206
- 23 JAPANESE STEWARTIA - 29
- 24 BALD CYPRESS - 7
- 25 BRODIE RED CEDAR - 29
- 26 BLACK GUM - 11
- 27 METASEQUOIA - 6
- 28 SOUTHERN MAGNOLIA - 15
- 29 NORWAY SPRUCE - 1
- 30 EASTERN REDBUD - 12
- 31 JAPANESE CRYTOMERIA - 3
- 32 HYDRANGEA, WAX MYRTLE, VIBURNUM
- 33 GRASSPAVE W/ BLUE GRAMA GRASS & WAVY HAIR GRASS
- 34 MOUNTAIN MINT
- 35 FOUNTAIN GRASS
- 36 GREEN VELVET BOXWOOD
- 37 AUTUMN MOOR GRASS
- 38 WARRIOR SWITCH GRASS
- 39 HARBOUR DWARF NANDINA
- 40 ANGELINA SEDUM
- 41 BLUE OAT GRASS
- 42 RIBBON GRASS
- 43 AUTUMN FIRE STONECROP
- 44 ROBB'S WOOD SPURGE
- 45 HUNGARIAN BEAR'S BREECHES & ICE DANCE JAPANESE SEDGE
- 46 HOSTA ELEGANS
- 47 HAIRY ALUM ROOT
- 48 OTTO LUYKEN LAUREL

- 49 ALICE OAKLEAF HYDRANGEA
- 50 INDIAN WOODOATS
- 51 YUCCA GLAUCA
- 52 25' FIRE TRUCK ACCESS BOUNDARY
-With grasspave, poured in place concrete, and concrete pavers occurring within boundary.



Phase I Illustrative Landscape Plan

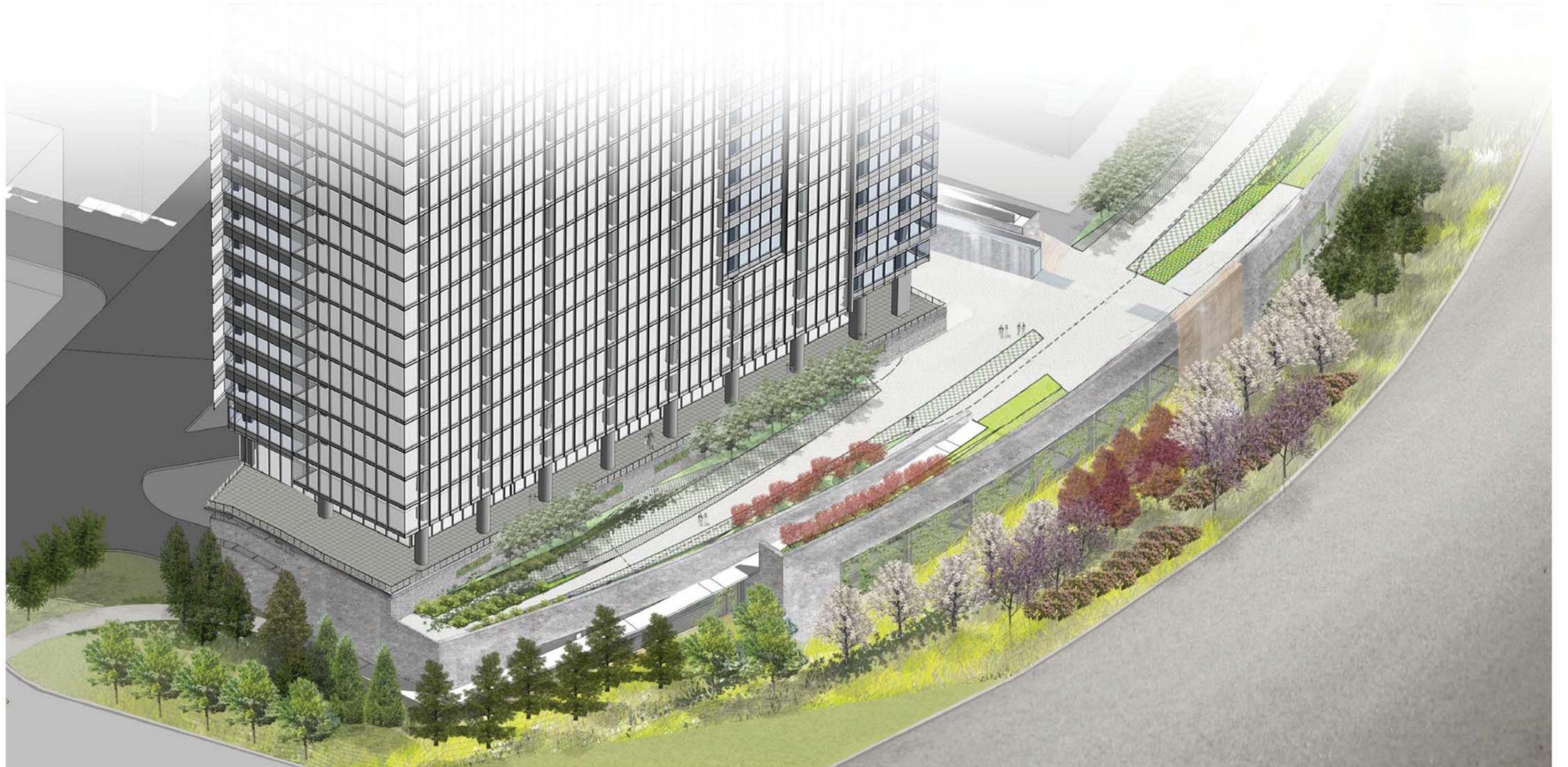




Landscaping illustrated represents 10 year growth

North Wall Elevation





Landscaping illustrated represents 10 year growth

Lower Plaza Axon

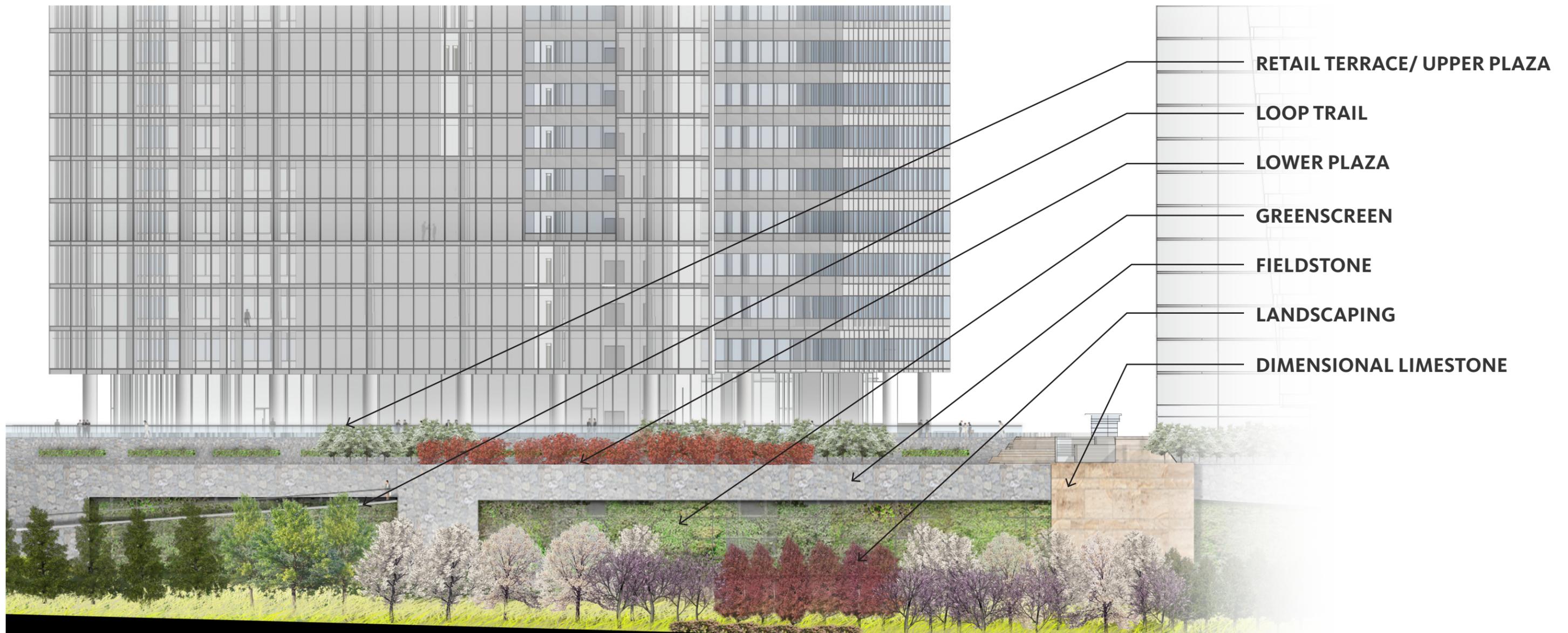
11.08.2011



Landscaping illustrated represents 10 year growth

North Wall Street View





Enlarged North Wall Elevation